



TITLE:

**Scrap Policy**  
**Acceptance Guidelines and Terms and Conditions**

Revision Date: 11/14/2024



# Scrap Policy

Revision Date: November 14, 2024



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**ALL SUPPLIERS OF SCRAP ARE REQUIRED TO CONFIRM REVIEW OF THIS POLICY ON AN ANNUAL BASIS BY RESPONDING TO THE ANNUAL SURVEYS ISSUED BY STELCO WITHIN SEVEN (7) DAYS OF RECEIVING SUCH SURVEY REQUESTS FROM STELCO.**

### INTRODUCTION

Stelco strives to source the highest quality ferrous scrap commodities and alternative iron units for our melt shop. Our valued suppliers play an integral role in our success. It is imperative that the quality of the ferrous scrap purchased is of a high standard and delivered in a timely manner to the mill. Non-conforming scrap (against the mill specification hereunder), will not be accepted.

The Stelco Scrap Policy outlines the acceptable quality and standards for ferrous scrap. Your efforts in applying these standards will ensure a long continuing relationship supplying ferrous scrap for our mill. All questions regarding the Stelco Scrap Policy should be directed to your Stelco commercial contact.

### Purpose:

The Stelco Scrap Policy defines the procedures of quality assurance and safe delivery practices required of our scrap suppliers. This includes safety guidelines, inspection procedures, and specification requirements for ferrous scrap.

### Responsibility:

1. Scrap suppliers are responsible for the safe and timely delivery of quality scrap to Stelco within agreed payment, shipping, and freight terms.
2. The Director – Raw Materials is responsible for the control, application, and maintenance of this policy. The Director – Quality Assurance is responsible for updating the policy when significant changes in the product and/or process have occurred.
3. The Scrap Auditor and Inspectors are responsible for ensuring safety procedures and scrap specifications are adhered to by suppliers.

### Revision History:

Date	Description of Change	Initials
09/30/2024	Reformatted policy, added General Specifications and Guidelines section, changed commodity specification layout for each type of scrap, added safety graphics.	MB
10/02/2024 and 10/31/2024	Proofread policy and made changes to grammar and information.	SR
11/14/2024	Added new sections including non-conforming material, truck inspection safety manual, and material loading best practices.	MB



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### SHIPPING TERMS AND CONDITIONS

1. Procurement of ferrous scrap is the responsibility of the Director – Raw Materials or their delegate.
2. Stelco reserves the right to cancel any and all unfulfilled contracts, providing notice to the Supplier.
3. Delivery may be by truck or rail.
4. Delivery may be made twenty-four hours per day, seven days a week. Stelco reserves the right to limit delivery hours per purchase order or by supplier at Stelco's sole discretion.
5. Stelco reserves the right to modify or update the Purchase Order General Terms and Conditions and the Stelco Scrap Policy.

### TRUCK – SHIPPING GUIDELINES

**Shipping Terms:** Delivery terms for ferrous scrap are DAP or DDP. Each rejection where any scrap is offloaded is subject to a \$500 reloading fee. The \$500 reloading fee will be allocated against the purchase order associated with the attempted delivery. Stelco scale ticket weights govern all deliveries and their associated payments.

**Delivery Period:** Material must be delivered no later than one week after the delivery date stated on the purchase order. Stelco reserves the right, at its sole discretion to review the status of the purchase order and reserves the right to cancel or continue deliveries until the purchase order obligation is completed.

Mandrel, sitter, pup, and bar coils are limited to two deliveries total per weekday per supplier. Over delivering these commodities are subject to rejection.

**Payment Terms:** Net 60 days from the date of receipt.

**Truck Equipment Requirements:** Scrap must be delivered in a trailer with side-hinged rear doors, an exception can be made for shred and coolant. Framed trailers are acceptable for all commodities. Frameless trailers are permissible for shredded and coolant scrap only. All truck drivers must be able to operate controls to raise and lower trailers from inside the cab. Supplier agrees that it shall be solely liable for any and all damages, including any clean up and/or down time resulting from supplier's utilization of any frameless trailers for delivery.

Customer Scrap (mandrel, slitter, pup, and bar coils) may be delivered on a flatbed. Flatbed drivers must complete and sign an UNLOADING DAMAGE WAIVER.



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### RAIL – SHIPPING GUIDELINES

**Shipping Terms:** Delivery terms for ferrous scrap are DAP or DDP. Shipments arriving at the mill without shipping notices may be assessed a \$500 fee. A rejected railcar may be assessed a \$750 fee.

Failure to notice rejected loads within 24 hours of rejection notice may result in demurrage fees which will be back charged to the supplier.

**Delivery Period:** Material must be delivered no later than one week after the delivery date month stated on the purchase order. Stelco reserves the right at its sole discretion to review the status of the purchase order and reserves the right to cancel or continue deliveries until the purchase order is completed.

**Payment Terms:** Net 60 days from the date of receipt.

**Rail Equipment Requirements:** All railcars must be fully open top railcars with vertical sidewalls and end walls with horizontal flat bottoms. Unacceptable railcars include v-troughs, belly troughs, or any interior ribs/framing. Scrap may be received in 65 ft length mill gondola cars. Coil gondolas are not accepted.

**Delivery Location Details:**

Consignee: Stelco Inc.

Rail Station: Nanticoke, ON

Final Rail Carrier: CN

Max Gross Car Limit: 263,000 lbs.

Min Car Limit: Not applicable.



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**SCRAP INSPECTION PROCEDURE**

**KEY CONSIDERATIONS**

- Please consider the safety of our inspectors when determining what to load on a truck
- Every truck or railcar which enters Stelco will pass through radiation detection and be inspected by trained inspectors at the scrap inspection stand.
- Truck entry is allowed upon presentation of a valid purchase order and proper shipping documents containing a radiation stamp/statement and authorized signature.
- If a load is flagged or vendor is on probation, the scrap inspector will direct the driver to dump the load in a segregated area of the yard for further inspection.
- If non-conforming material is found, the scrap inspector will inform supervision for a second inspection. If the supervisor agrees with the scrap inspector, pictures of the non-complying material will be taken, and a FULL rejection notice will be issued to the supplier via email. The notice will include photos, and the reason for the rejection.
- If the scrap load is accepted at the stand, the truck driver will be directed by Stelco personnel to unload at the field, yard, or door 10. Once the truck driver has dumped the scrap on the ground, the Inspector will perform a walkaround to look for non-conforming scrap. If pieces of non-conforming scrap are found, they will be loaded back onto the truck to be returned to the supplier and a PARTIAL rejection notice will be issued to the supplier.
- If majority of the load is non-conforming, then the entire load will be loaded back onto the truck and a FULL rejection notice will be given to the driver and sent to the supplier via email. The notice will include clear photos and the reason for the rejection.
- A \$500 reloading fee will be charged and noted on the purchase order in the event any non-conforming scrap is reloaded. For trucks Stelco reloads with non-conforming scrap, a DAMAGE WAIVER will be completed and signed by the driver. It is at Stelco's sole discretion to expedite the dumping of trucks in queue.

**UNPLANNED DELIVERIES**

- The Scrap Inspector calls their supervisor and will determine if the load can be accepted based on the discretion of the supervisor. The supplier will be made aware that they have an unscheduled load.
- Mandrel, slitter, pup, and bar coils of which under normal circumstances two loads total per weekday can be accepted. Any additional shipments may be received at the discretion of the Scrap Inspector and their supervisor.



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### TRUCK DRIVER DELIVERY INSTRUCTIONS

All drivers must comply with the following instructions. **Your Safety Is Your Personal Responsibility.** Failure to comply with the instructions may result in a FULL rejection and/or suspension from Stelco property.

#### TRUCK DRIVER INSTRUCTIONS

1. A valid purchase order is required for entry to Stelco's site. A non-valid purchase order will result in a full rejection. If the delivery exceeds the purchase order quantity, the truck will be rejected entry.
2. Upon arriving at the Scrap Yard, wait at the inspection platform for instruction and inspection from the Scrap Inspector. If another truck is already waiting at the platform, take your place in the queue.
3. Come to a full stop at all railway crossings and if visibility is obstructed by steam or fog.
4. Be aware of audio warning devices – trains, auto, mobile equipment back-up, cranes, plant start up, etc.
5. Do not park on or near a railroad track. Maintain a 10ft clearance from the centerline of the tracks at all times.
6. Remain in the vehicle until the scrap inspector directs the driver to the designated dumping area.
7. DO NOT back up until a spotter is in place and providing direction.
8. Ensure truck back up alarm is operable.
9. Obey all instructions of the Scrap Inspector, both verbal and hand signals.
10. Railway Locomotives, Heavy equipment, and Pedestrians have the right of way. Stay at least 50 feet back from all mobile equipment. Continually check your surroundings and never position yourself in the blind spots or swing radius of mobile equipment (material handler, pot hauler, etc.) Do not move near equipment that has not acknowledged your presence. Stay out of tipping radius of any trailer that raises its bed to dump.
11. Back into the approximate area indicated by the Scrap Inspector.
12. Use extreme caution when opening trailer doors and ensure there are no tripping hazards or personnel in the immediate area. While opening the door, ensure you are out of the path of any potential fall hazards such as scrap that has shifted during transport.
13. Ensure the truck is on level ground prior to unloading scrap. Refer to diagram on Page 10.
14. Prior to dumping, ensure visual contact with the Scrap Inspector is maintained until unloaded.
15. After unloading, lower the trailer and pull ahead out of the scrap dump location.
16. Wait for the Scrap Inspector to ensure the load is acceptable. Any non-conforming scrap will be loaded back into the truck through the use of on-site scrap cranes. If the load is deemed to be conforming, the driver can collect their signed paperwork, close the trailer doors, and exit the scrap yard.
17. Anticipate unloading delays in bad weather (lightning, heavy snow, rain, and periods of high winds and gusts).
18. If an evacuation alarm sounds, remain in the truck until further instructions.
19. The following Personal Protective Equipment must be worn while on site:
  - a. HARD HAT
  - b. SAFETY GLASSES WITH SIDE SHIELDS
  - c. LONG PANTS. NO SHORT PANTS
  - d. CLOSED-TOE SAFETY SHOES—steel-toed boots or metatarsals
  - e. HIGH VISIBILITY VEST—Must be worn when outside the truck.
20. Follow all Stelco in-plant traffic requirements. Obey the speed limit signs (where no signage is posted, the limit is 45km/h).

#### PROHIBITED

21. NO alcohol or drugs are permitted on Stelco property.
22. Drivers must not be under the influence of drugs or alcohol. In the event a driver is involved in a safety incident, Stelco has the right to test for drugs and alcohol.
23. NO passengers or pets are allowed OUTSIDE THE CAB of the truck (passengers must be 18yrs of age or older)
24. NO cell phone use including hands-free or text messaging WHILE DRIVING. PULL OVER TO USE devices.
25. Drivers MUST wear seat belts when driving in the plant.
26. Firearms, knives, or other weapons are NOT allowed on the plant site.



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# Driver PPE & Safety

**DRIVERS MUST READ THIS CARD BEFORE UNLOADING AT ANY STELCO FACILITY**










**Think Safe, Work Safe**

## P.P.E (PERSONAL PROTECTIVE EQUIPMENT)

1. **HARD HAT** – Must be **WORN AT ALL TIMES**
2. **GLASSES** – Must wear **SAFETY GLASSES WITH SIDE SHIELDS**
3. **SHIRT** – Short sleeve shirt with a minimum of a 4" sleeve
4. **LONG PANTS** – Are required, **NO SHORT PANTS WHILE ON SITE.**
5. **CLOSED-TOE SAFETY SHOES** – Steel-toed boots or metatarsals **WHILE ON SITE**
6. **HIGH-VISIBILITY REFLECTIVE VEST** – Must be worn **WHEN OUTSIDE THE TRUCK**



## DRIVING ON STELCO PROPERTY

- Drivers **MUST** obey speed limit signs. Where no signage is posted, 45 KM/H is the maximum speed. 
- Drivers **MUST** come to a full stop at all  signs and railroad tracks.
- Trucks **MUST NOT** park or stop on railroad  tracks and must park at least 15 ft from tracks.
- **NO** alcohol or drugs are permitted on STELCO property. 
- **NO** passengers  or pets  are allowed **OUTSIDE THE CAB** of the truck upon entering the main gate.
- Railroad  and pedestrian  traffic **ALWAYS** have the right of way.
- **NO** talking on cellular phones, hands-free, or text messaging  **WHILE DRIVING** on STELCO premises. **PULL OVER TO USE** these devices.
- Drivers **MUST STAY 10 FT AWAY** from railroad cars    at all times.
- Drivers **MUST** wear seat belts  when driving in the plant.
- Firearms  or other weapons of any kind are **NOT** allowed on STELCO sites.

**It is the driver's responsibility to arrive on site with all required PPE. Failure to adhere to any of the above safety requirements will result in shipment not being unloaded and may jeopardize future business with Stelco.**





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### TRAILER TURN-OVER PREVENTION AND INSPECTION POINTS

#### **DUMP TRAILER TURN OVER PREVENTION: NO EXCEPTIONS ZERO TOLERANCE**

- Maintain a minimum 50ft. clearance around the perimeter of the truck.
- Ensure truck is parked on solid level ground (cannot be soft or sloped).
- Before dumping, verify that the load has not shifted in transit.
- Do not dump or raise the trailer for any reason during high gusty winds.
- Ensure tires are in good shape, without cuts or damage.
- Preloading scrap between November 1 to March 30 is not permitted. Material can freeze and pose a serious hazard.
- *It is the driver's responsibility to determine if the load is safe to dump. Failure to adhere to any of the above requirements will result in not being unloaded and jeopardize future business.*

#### **DUMPING EQUIPMENT SAFETY: CRITICAL INSPECTION POINTS**

##### **ROLL-OFF TRAILERS & DUMP TRAILERS**

- Check all welds for breaks and/or corrosion.
- Maintain (grease) and inspect all hinges.
- Maintain (grease) and inspect all chains/binders.

##### **REAR SUSPENSION/TRAILER FRAME**

- Check all airbags for proper inflation (free of leaks)
- Ensure all mounting brackets are free of corrosion.
- Ensure all lines to airbags are in good condition.
- Ensure there are no broken springs, spring center bolts, or shackle bolts.
- Maintain all hinge/dump pins (Lubricated, free of cracks and/or excessive wear)
- Ensure tires are properly inflated. Ensure rims/wheels are free of cracks, breaks and missing/broken lug nuts.
- Ensure the frame of the dump trailer or roll-off container has no breaks in the frame. Ensure all bolts and welds are in good condition and properly secure the trailer or roll-off frame to the chassis of the trailer.

##### **DUMP TUB/TRAILER FRAME**

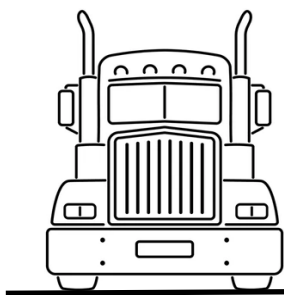
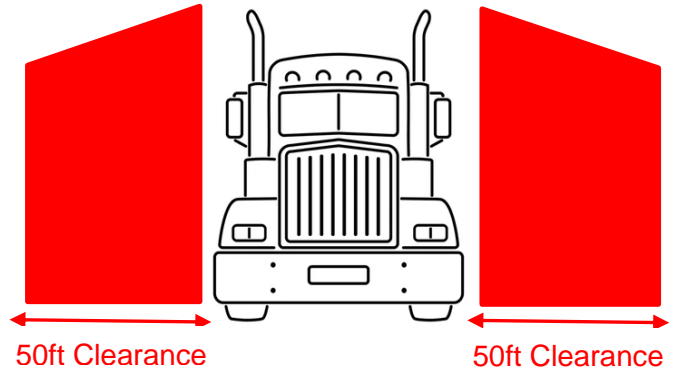
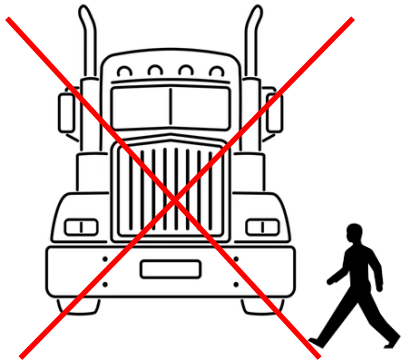
- Maintain dump tub and ensure it is free of breaks, cracks, broken welds, and broken mounting brackets.
- Ensure all cylinder mounts are properly lubricated and free of cracks or breaks.
- Ensure all cylinder pins are visibly in place and not working their way out of the mounts.
- Inspect all hydraulic cylinders, fittings, hoses, and valves for blown, bulging or leaking seals.
- Ensure all rollers on a roll-off trailer have a retaining bolt or cotter key to prevent the loss of a roller.

##### **LANDING GEAR & FIFTH WHEEL AREA**

- Inspect all landing gear bolts, brackets, and welds for excessive wear and/or corrosion. Ensure all bolts are tight, not allowing the landing gear to move while in use.
- Ensure fifth wheel plate is free of cracks and that the fifth wheel pin is secure and in good condition.
- Check hydraulic fittings for cracks, leaks, or missing parts.

Please ensure that all drivers have reviewed and understand the above instructions prior to delivering scrap to Stelco.  
Please ensure that all drivers retain a copy of this document while on STELCO property.

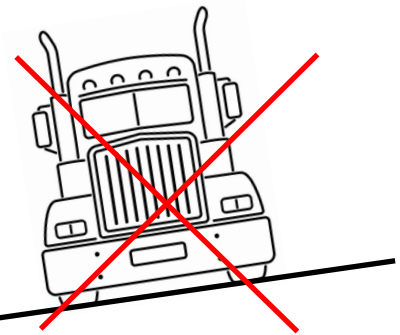
## Dump Trailer Turn-Over Prevention



Solid – Level  
**SAFE**



Soft Ground  
**UNSAFE**



Sloped – Not Level  
**UNSAFE**



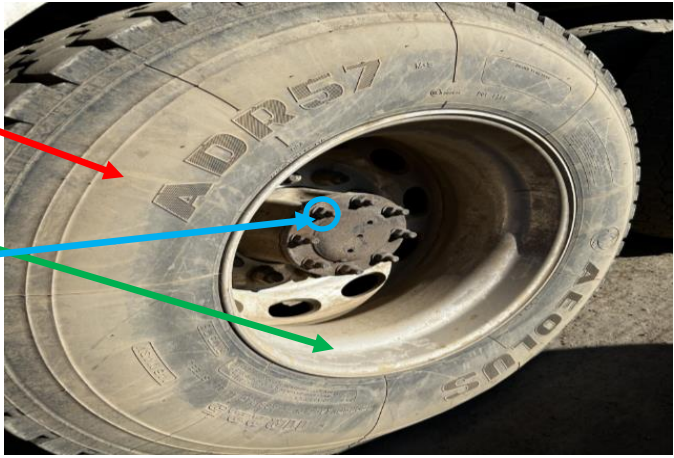

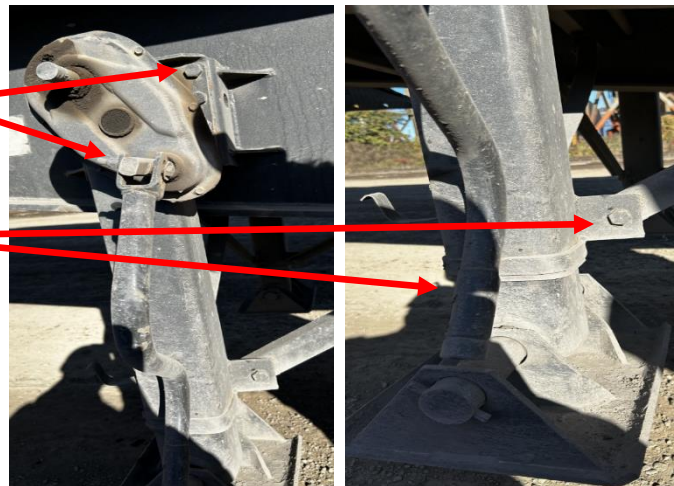
Before dumping, verify that the load has not **SHIFTED** in transit.



**DO NOT** dump or raise the trailer for any reason during high gusty winds.

**It is the driver's responsibility to determine if the load is safe to dump. Failure to adhere to any of the above safety requirements will result in shipment not being unloaded and may jeopardize future business with Stelco.**

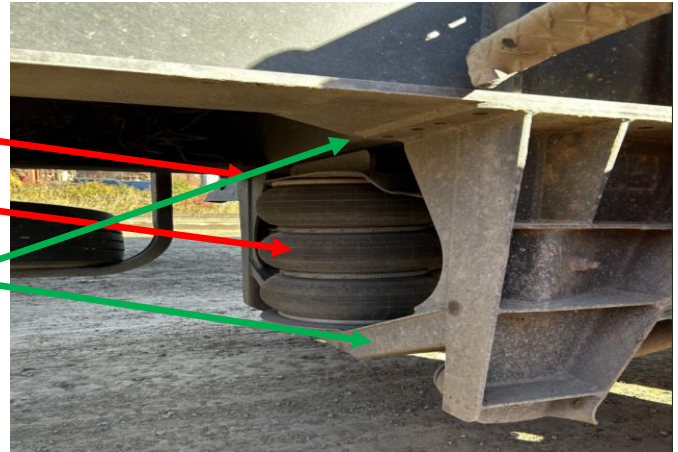
**TRUCK SAFETY INSPECTION MANUAL**

		LOCATION ON VEHICLE: SIDES
<p><b><u>Tires:</u></b></p> <ul style="list-style-type: none"> <li>• Ensure tires are properly inflated</li> <li>• Ensure rims/wheels are free of cracks, breaks, and broken lugnuts</li> <li>• Ensure no lugnuts are loose</li> </ul>		
<p><b><u>Trailer Frame:</u></b></p> <ul style="list-style-type: none"> <li>• Ensure frame of the dump trailer or roll-off container is free of breaks/cracks</li> <li>• Ensure all bolts and welds are in good condition and are properly securing the frame to the chassis of the trailer</li> </ul>		
<p><b><u>Landing Gear:</u></b></p> <ul style="list-style-type: none"> <li>• Ensure all bolts are tight, preventing movement of landing gear during use</li> <li>• Inspect all bolts, brackets, and welds for excessive wear and corrosion</li> </ul>		

**LOCATION ON VEHICLE: SIDES**

**Airbag:**

- Ensure airbag is free of cracks and excessive wear before use
- Ensure airbag is properly inflated
- Ensure mounting brackets are free of corrosion
- Ensure lines to airbag are free of chafing or leaks



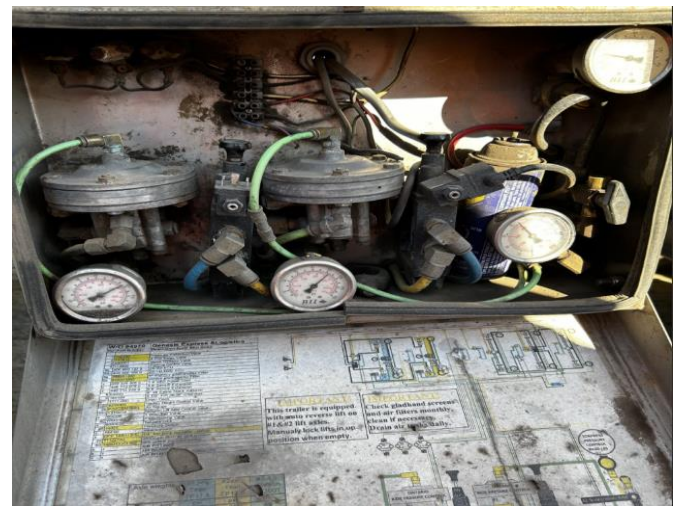
**Brakes:**

- Ensure brake pads and shoes are in good condition
- If dump truck contains drum brakes, check wheel cylinders for damage
- Ensure lines and hoses are not damaged



**Lift Box:**

- Ensure all gauges are at appropriate levels for lowering and lifting the suspension



**LOCATION ON VEHICLE: SIDES**

**Loose Wheel Nut Indicators:**

- Ensure there are no tears on rubber indicators
- Rubber tearing will indicate if wheel nuts are turning or loosening



**Lift Axle Bracket and Suspension:**

- Check lift axle bracket for cracks or damage
- Check suspension for any damage



**Safety Bar (Barn Door Style Trailer):**

- Ensure safety bar is firmly resting against the side of the trailer
- Ensure chains and safety pin are attached to the safety bar



**LOCATION ON VEHICLE: REAR**

**Trailer Box:**

- Check for cracks or damage in the trailer frame



**Main Door:**

- Before dumping, make sure safety latches are pointing diagonally away from the door
- Ensure chains are hooked to main door



**Primary Door Hinges:**

- Ensure all hinges are fully secured
- Check if any hinges are loose and misaligned from other hinges



**LOCATION ON VEHICLE: REAR**

**Taillights:**

- Ensure taillights are functioning and are clearly visible to nearby workers and designated spotter when dumping



**Main Door (Gate Style Trailer):**

- Inspect hinges for damage
- Inspect frame and welds for breaks or corrosion



**Safety Bar (Gate Style Trailer):**

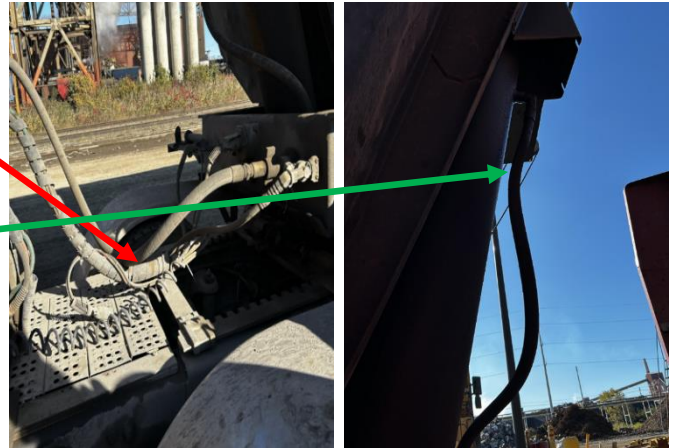
- Ensure safety chain is firmly against trailer frame
- Inspect safety bar, brackets, and bolts for any damage or corrosion



**LOCATION ON VEHICLE: SIDES**

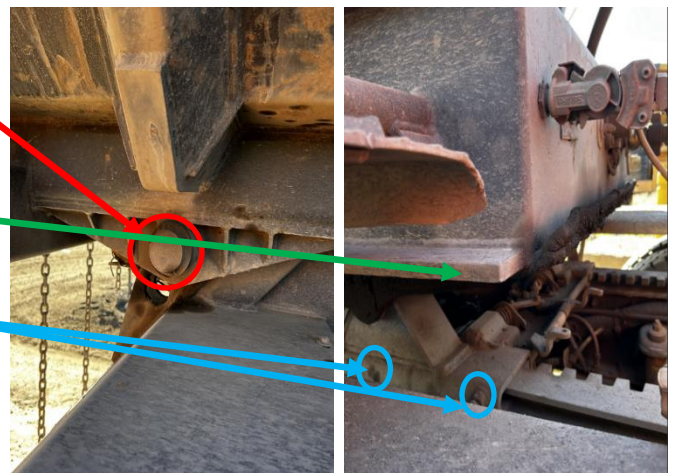
**Hoist:**

- To avoid heat damage, ensure hoses do not rest against the frame of the truck
- Before dumping, check hoist for any cracks or leaks



**Fifth Wheel Plate, Hinge, and Pins:**

- Ensure hinge is properly secured and not excessively corroded
- Ensure underside of fifth wheel plate is free of cracks
- Ensure pins are secured and in good condition



**Pull Cable:**

- Inspect pull cable for excessive wear





**MATERIAL LOADING BEST PRACTICES**

**BEST PRACTICES**

- Avoid unnecessary voids when loading the trailer.
- Scrap must be loaded with a grapple, magnet, or loader depending on material.
- Ensure material is evenly distributed across the dump bed.
- Avoid piling excess material in one area, as this may cause debris to hang over the trailer and pose a potential safety hazard.

**EXAMPLES OF LOADS**

**Industrial/Prime Factory Bundles:**



- Evenly distributed across dump bed
- No voids
- No material piled up in a single area
- No material hanging over dump bed

**Heavy Melt:**



- No material piled up in a single area
- Unnecessary void, can pose a tipping hazard **(AVOID)**

**Heavy Melt:**



- No material piled up in a single area
- Unnecessary void, can pose a tipping hazard **(AVOID)**

**Rebar:**



- Evenly distributed across dump bed
- No voids
- Slight pile up of material in a single area (can pose a hazard when dumping)

**RHOB Coolant:**



- Evenly distributed across dump bed
- No voids
- No material piled up in a single area
- **Partial void is allowed** due to dump bed carrying a smaller load

**Bushelling:**



- Evenly distributed across dump bed
- No voids
- No material piled up in a single area
- No material hanging over dump bed

**Plate and Structural:**



- **Partial void** (can pose hazard when dumping)
- **Pile up of material** in a single area (can pose hazard when dumping)

**Shred:**



- Evenly distributed across dump bed
- No voids
- No material piled up in a single area
- No material hanging over dump bed



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### GENERAL SCRAP ACCEPTANCE GUIDELINES

This list is not all inclusive. Other items not listed may be inappropriate for recycling as scrap metal. Suppliers must provide Stelco with documentation showing ALL SCRAP SUPPLIER Employees and Truck Drivers have been trained and have reviewed the Scrap Policy. Remember that any load or part thereof may be rejected at your cost if guidelines are not followed.

### **FERROUS SCRAP GENERAL ACCEPTANCE GUIDELINES**

#### **Definitions:**

- **Free of:** When specified to be “free of”, the grade shall not contain any amount of prohibited material.
- **Free of alloys:** Scrap is “free of alloys” if the shipment is free of highly alloyed steels such as stainless or leaded steels. An example of residual content of the elements is listed below, contained within the steel. The combined residual level, excluding manganese, shall not exceed 0.50%. Element residual levels within the Scrap shall not exceed:
  - Chromium (Cr) 0.20%
  - Nickel (Ni) 0.30%
  - Manganese (Mn) 1.5%
  - Molybdenum (Mo) 0.03%
- **Grade or Type:** A classification of scrap type for those not within this policy, is based on the Institute of Scrap Recycling Industries (ISRI) ferrous scrap classifications.

#### **General Specifications**

- **Cleanliness:** All scrap grades shall be free of dirt, nonferrous metals or foreign material of any kind, excessive rust, and corrosion.
- **Quality:** All material is to be suitable and acceptable to Stelco.
- **Loading/Unloading:** All grades must be capable of being loaded and unloaded by electromagnet or grapple.



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### GENERAL SPECIFICATIONS—CONTINUED

#### Unacceptable Materials

- **Hazardous Materials:** All grades shall be free of hazardous materials including but not limited to:
  - **Asbestos or asbestos-containing material** such as pipe insulation and surfacing material commonly found on I-beams, tanks, and other structural and demolition debris.
  - **Chlorinated Plastics:** Materials that contain chlorinated plastics include but are not limited to:
    - Consumer products (e.g., carpeting, clothing, foam, furniture, Styrofoam)
    - Containers (e.g. bags, bottles, food packaging, packaging materials)
    - Plastic motor vehicle components (e.g. battery casings, bumpers, plastic parts)
  - **PCB-containing Material (Polychlorinated Biphenyls):** Items that contain or have contained PCBs: including small capacitors, fluorescent light ballasts, and electrical transformers or transformer components.
  - **Refrigerants** or refrigerant-containing materials (including CFCs and HCFCs) in refrigerators and air conditioners.
  - **Military Scrap:** Military scrap of any kind, unless approved in advance. This includes ammunition casings, munitions, bullets, and military target range scrap.
  - **Explosives** (or any explosive residues).
  - **Ignition Sources, Flammable or Combustible materials.**
  - **Fracking Scrap:** All grades are to be free of gas and oil well perforation tools/piping and all other hazardous materials (e.g.) explosives associated with fracking.
  - **Hazardous Wastes,** Other regulated wastes, and their containers.
  - **Oils, gasoline, other petroleum products and antifreeze:** Includes hydraulic fluids, gear oils and grease. Hydraulic equipment must have hydraulic hoses removed and cylinders cut open and drained.
  - **Oil-filled devices** (e.g. capacitors or transformers) and free-flowing oil (on surface or containerized)
  - **Lead Containing Items:** Potential lead containing items such as babbitt bearings, counterweights of any kind, sewer pipe connections, hexagonal rods (spark plug stock), terne plate, wheel weights, and items coated in lead paint.
  - **Batteries:** Lead-acid, Lithium-ion, Nickel-metal hydride, Lithium-sulfur batteries, or battery parts
  - **Electronic waste:** Computers, monitors, instrumentation panels
  - **Mercury containing** materials including but not limited to mercury-wetted or containing relays, switch gear, rectifiers, gauges, manometer, barometers, thermometers, and fluorescent lighting fixtures.
  - **Tires** or tire pieces of any type
  - **Nonmetallic material** (wood, paper, fibers, rubbers, foam, fluff, insulation, concrete, dirt, yard debris, plastic, oils, greases, asphalt, glass etc.).
  - **Nonferrous materials** (electric motors, piping, compressors, wires, brass/bronze castings, copper plate bundles, tin plated materials, lead, aluminum etc.).
- **RADIOACTIVE Materials:** Radioactive materials of any kind are strictly prohibited. All inbound scrap by truck and railcar is scanned by radiation detection equipment.
  - **A separate document, A0-025-081-039 - Stelco Radiation Protection Policy for Purchased Ferrous Metals, must be signed by Suppliers.**
- **All Sealed Containers, Tanks, Barrels, and Drums:** These items should not be recognizable as containers: They shall be processed or shredded, so they are not recognized as a container.
  - All grades shall not contain Closed, Enclosed, or Sealed containers. Such containers are a serious explosion hazard, which may result in the loss of life, limb, and /or property. Any scrap shipment arriving that is found to contain a closed container will be rejected. Containers may only be accepted provided they have been processed such that one end has been removed, a minimum of two large holes breach the “sealed” portion of the container, or the container has been cut in half.



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### GENERAL SPECIFICATIONS—CONTINUED

- Closed, Enclosed, or Sealed Containers include, but are not limited to, hydraulic or gas cylinders, compressed gas cylinders or tanks, oxygen or propane tanks, fire extinguishers, aerosol cans that have not been properly vented or depressurized, or any vessels which contains or retains gases, oils, fluids or pressurized substances, sheared pipe that is not open on both ends, auto/truck drive shafts, shock absorbers, struts, torque converters, gear boxes, conveyor rollers, closed ball valves, etc.
- **Stainless Steel:** All grades shall be free of stainless steel
- **Cast Iron:** All grades shall be free of cast iron.
- **Tin Plate:** All grades shall be free of tin-plated materials.
- **Residual Alloys:** Wherever the term “free of alloys” is used in the classifications given herein, it shall pertain to highly alloyed steels such as stainless or leaded steels.
- **Free Organic Liquids:** Materials that contain free organic liquids include, but are not limited to, oil filters engine blocks, oily turnings, oily machine shop borings, or storage drums or totes.
- **Counterweights** of any kind processed or unprocessed in any scrap types.
- **Crimped/Pinched ends for pipe and tubing:** Materials must be open on both ends from November 1 through March 31. From April 1 through September 30 one end must be open. The pipe opening must be large enough in size to guarantee water cannot be trapped and will easily flow out. If the pipe has been flattened, both ends can be pinched any time throughout the year.
- **Gas or hydraulic cylinders and other sealed containers:** Sealed containers are described as air or watertight containers without visible openings. Cylinder barrels must have one end cut off. The cylinder opening must be large enough in size to guarantee water cannot be trapped and will easily flow out. Simply knocking off the small valve is not sufficient; the end must be cut off. Intact Gear Cases, Hydraulic or Fluid pumps (such as power steering pumps)
- **Vehicle airbags** or air bag actuators, especially those containing sodium azide.
  - Air bags from vehicle suspension systems must be removed from axels.
- **Paint cans or other paint containers.**
- **Machine shop parts;** (die blocks, hardened materials, machine bases)
- **Lights:** Fluorescent lights, neon, high intensity, or mercury vapor lights.
- **Fines:** Fines include mill scale, B scrap, turnings, kish, dirt or other debris such as concrete and gravel



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### #1 Industrial/Prime Factory Bundles

<b>Specifications</b>	<p><b>Sizing Dimensions:</b> L x W x H</p> <p><i>Maximum:</i> 1.0m x 0.76m x 0.76m (3ft x 2.5ft x 2.5ft)</p> <p><i>Minimum:</i> 0.76m x 0.76m x 0.76m (2.5ft x 2.5ft x 2.5ft)</p> <p><b>Minimum Density:</b> 1600 kg/m<sup>3</sup> (100 lb/ft<sup>3</sup>)</p>
<b>Description:</b>	New black or galvanized steel scrap, clippings or skeleton scrap prepared via hydraulic baler. Bundles must be suitable for handling with a magnet and must maintain their form when handled multiple times.
<b>Do Not Include:</b>	<p>Torch drippings; Old auto body or fender stock; tin material; coated, limed, or vitreous enameled metals; electrical sheet containing over 0.5% Si; painted material.</p> <p>Evidence of wrapping bundles with the intent to conceal lesser quality, dangerous, or nonconforming material will result in rejection.</p>
<b>Acceptance Criteria:</b>	Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.
<b>Approval:</b>	Authorized suppliers of key commodities must obtain prior approval from STELCO whenever the product or process is different from that currently approved.

Alloy	%, Max
Phosphorus	0.03
Sulfur (S)	0.025
Copper (Cu)	0.12
Nickel (Ni)	0.12
Chrome (Cr)	0.12
Molybdenum (Mo)	0.03
Tin (Sn)	0.03
Vanadium (Va)	0.01
Zinc (Zn)	0.30
Silicon (Si)	0.50
Carbon (C)	0.30
Manganese (Mn)	1.00





### #1 Dealer Bundles

<b>Specifications</b>	<p><b>Sizing Dimensions:</b> L x W x H</p> <p><i>Maximum:</i> 1.0m x 0.76m x 0.76m (3ft x 2.5ft x 2.5ft)</p> <p><i>Minimum:</i> 0.76m x 0.76m x 0.76m (2.5ft x 2.5ft x 2.5ft)</p> <p><b>Minimum Density:</b> 1400 kg/m<sup>3</sup> (87 lb/ft<sup>3</sup>)</p>
<b>Description:</b>	New and old black, galvanized, or painted steel sheet scrap prepared via hydraulic baler. Bundles must be suitable for handling with a magnet and must maintain their form when handled multiple times. May contain used siding, auto body sheets, bare fencing, and cables.
<b>Do Not Include:</b>	Non-metallics, rebar, tin or lead coated material, barrels, and drums.  Evidence of wrapping bundles with the intent to conceal lesser quality, dangerous, or nonconforming material will result in rejection.
<b>Acceptance Criteria:</b>	Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.
<b>Approval:</b>	Authorized suppliers of key commodities must obtain prior approval from Stelco whenever the product or process is different from that currently approved.

Alloy	%, Max
Phosphorus	0.05
Sulfur (S)	0.05
Copper (Cu)	0.35
Nickel (Ni)	0.20
Chrome (Cr)	0.20
Molybdenum (Mo)	0.05
Tin (Sn)	0.03
Vanadium (Va)	0.10
Zinc (Zn)	0.30
Silicon (Si)	0.50
Carbon (C)	0.30
Manganese (Mn)	1.00



## #1 Busheling

<b>Specifications</b>	<i>Maximum:</i>	1.5m spec (5ft x 2ft) 0.9m spec (3ft x 2ft) 0.3m spec (1ft x 1ft)
	<i>Minimum (For All Specs):</i>	0.15m x 0.15m 6" x 6" sheet (No steel shavings)
	<b>Thickness:</b>	Maximum: ¼" Minimum: ⅛"
	<b>Minimum Density:</b>	1.5m spec: 640 kg/m <sup>3</sup> or 40lb/ft <sup>3</sup> 0.9 m spec: 960 kg/m <sup>3</sup> or 60lb/ft <sup>3</sup> 0.3 m spec: 1600 kg/m <sup>3</sup> or 100lb/ft <sup>3</sup>
<b>Description:</b>	New, clean alloy-free 1000 series/HSLA carbon steel clippings and/or stampings commonly generated in automotive manufacturing. May include up to 25% skeleton type material by volume.	
<b>Do Not Include:</b>	Obsolete scrap of any kind. Slitter coils, mandrel, bar, or pup coils. Old auto body and fender stock; closed containers; coated, limed, or vitreous enameled metals; electrical sheet containing over 0.5% Si, excessively greasy or oily materials. Painted material, Mill scale, Slag, Non-metallics, Non-ferrous, Steel mill byproducts of any kind.	
<b>Acceptance Criteria:</b>	Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.	
<b>Approval:</b>	Authorized suppliers of key commodities must obtain prior approval from Stelco whenever the product or process is different from that currently approved.	

Alloy	%, Max
Phosphorus	0.03
Sulfur (S)	0.025
Copper (Cu)	0.12
Nickel (Ni)	0.12
Chrome (Cr)	0.12
Molybdenum (Mo)	0.03
Tin (Sn)	0.03
Vanadium (Va)	0.01
Zinc (Zn)	0.30
Silicon (Si)	0.50
Carbon (C)	0.30
Manganese (Mn)	0.80





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### #1 Heavy Melt

<b>Specifications</b>	<p><b>Sizing Dimensions:</b> L x W x H</p> <p><i>Maximum:</i> 0.9m x 0.45m x 0.3m (3ft x 1.5ft x 1ft)</p> <p><i>Minimum:</i> 0.15m x 0.15m x 0.15m (6" x 6" x 6")</p> <p><b>Minimum Density:</b> 700 kg/m<sup>3</sup> (43 lb/ft<sup>3</sup>)</p>
<b>Description:</b>	Clean, mild steel scrap and/or wrought, black or galvanized iron over ¼ inch in thickness obsolete or new. May contain truck & auto rims, may contain Heavy walled pipe, hydraulic cylinders must be separated from the ram, up to 10% of the following auto scrap is allowed; gears, axles, shafts, bearings, springs evenly distributed throughout load.
<b>Do Not Include:</b>	Cast iron and/or cast pipes (sewer grates, manhole covers, brake rotors); ingots, blooms, blanks ,tubing over 10" in diameter unless split in half across circumference, some automobile scrap including , crankshafts, motor blocks, gas tanks, etc.; turnings or borings; coated materials such as ,but not limited to, tin plate, lead plate, vitreous enameled, etc.; recuperated tubing and heat exchanger piping; electrical equipment such as, but not limited to, motors, transformers, electrical switches, gear, etc.; wire rope not cut to 1 m lengths; gears and shafts; sheet iron; rolls; bearings; barrels and drums. Lead weights must be removed on truck / auto rims.
<b>Acceptance Criteria:</b>	Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.
<b>Approval:</b>	Authorized suppliers of key commodities must obtain prior approval from Stelco whenever the product or process is different from that currently approved.

Alloy	%, Max
Phosphorus	0.05
Sulfur (S)	0.05
Copper (Cu)	0.35
Nickel (Ni)	0.20
Chrome (Cr)	0.20
Molybdenum (Mo)	0.05
Tin (Sn)	0.03
Vanadium (Va)	0.10
Zinc (Zn)	0.00
Silicon (Si)	0.50
Carbon (C)	0.40
Manganese (Mn)	1.50





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### Pit Scrap

<b>Specifications</b>	<p><b>Sizing Dimensions:</b> L x W</p> <p><i>Maximum:</i> 0.076m x 1.2m (3" x 4ft)</p> <p><i>Minimum:</i> N/A</p> <p><b>Minimum Density:</b> N/A</p>
<b>Description:</b>	Steel pit scrap generated from steelmaking operations. Seller must provide guaranteed minimum percent of iron (Fe). Seller must provide calculations used to perform iron content analysis.
<b>Do Not Include:</b>	Residual alloys, Excessive slag contamination; flue dust; refractory materials such as brick, mortar, gunnite, etc.
<b>Acceptance Criteria:</b>	Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.
<b>Approval:</b>	Authorized suppliers of key commodities must obtain prior approval from Stelco whenever the product or process is different from that currently approved.

Alloy	%, Max
Phosphorus	0.03
Sulfur (S)	0.03
Copper (Cu)	0.20
Nickel (Ni)	0.20
Chrome (Cr)	0.20
Molybdenum (Mo)	0.05
Tin (Sn)	0.03
Vanadium (Va)	0.10
Zinc (Zn)	0.00
Silicon (Si)	0.50
Carbon (C)	0.40
Manganese (Mn)	2.00





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### Plate and Structural (P&S)

<b>Specifications</b>	<p><b>Sizing Dimensions:</b> L x W x H</p> <p><i>Maximum:</i> 0.45m x 0.12m x 0.45m (2ft x 4ft x 2ft)</p> <p><i>Minimum:</i> 0.15m x 0.15m x 0.15m (6" x 6" x 6") (No steel shavings)</p> <p><b>Minimum Density:</b> 750 kg/m<sup>3</sup> (47 lb/ft<sup>3</sup>)</p>
<b>Description:</b>	Cut structural and plate scrap, 4 feet and under in length, clean steel plates, black or galvanized iron over ¼" thickness obsolete or new, structural shapes, crop ends, shearings, or broken rail wheels. Not less than ¼" thickness. *Note: Maximum amount of skeleton plate is 25% by weight per load.
<b>Do Not Include:</b>	HMS, Ingots, Blooms, blanks tubing over 10" in diameter unless split in half across circumference; closed containers of any kind such as, but not limited to, crimped cylinders (any potential closed container must have an opening on at least one end that is sufficient to allow liquid to flow freely from the piece of scrap; see seasonality of crimped ends in General Scrap Acceptance Guidelines); automobile or farm equipment scrap; cast iron or cast pipe; recuperated tubing or heat exchanger piping; turnings; borings, steel wheel rims; barrels and drums. Must be free of non-ferrous and non-metallic material.
<b>Acceptance Criteria:</b>	Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.
<b>Approval:</b>	Authorized suppliers of key commodities must obtain prior approval from Stelco whenever the product or process is different from that currently approved.

Alloy	%, Max
Phosphorus	0.05
Sulfur (S)	0.05
Copper (Cu)	0.25
Nickel (Ni)	0.15
Chrome (Cr)	0.15
Molybdenum (Mo)	0.03
Tin (Sn)	0.03
Vanadium (Va)	0.01
Zinc (Zn)	0.30
Silicon (Si)	0.50
Carbon (C)	1.20
Manganese (Mn)	1.50





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### Rail and OTM (Other Track Material)

<b>Specifications</b>	<p><b>Sizing Dimensions:</b></p> <p><i>Maximum:</i> 1200m (4ft) length for yard</p> <p><i>Minimum:</i> N/A</p> <p><b>Minimum Density:</b> 25 kg/m (17 lb/ft<sup>3</sup>)</p>
<b>Description:</b>	<p>Rail Crops: Rail steel crops standard section 4 ft long and under.          OTM: tie plates, spikes.          Rail Crops and OTM material are not to be mixed together.</p>
<b>Do Not Include:</b>	<p>Manganese frogs and switch points, railcar components of any kind, cast iron car wheels, and axles.</p>
<b>Acceptance Criteria:</b>	<p>Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.</p>
<b>Approval:</b>	<p>Authorized suppliers of key commodities must obtain prior approval from Stelco whenever the product or process is different from that currently approved.</p>

Alloy	%, Max
Phosphorus	0.05
Sulfur (S)	0.07
Copper (Cu)	0.15
Nickel (Ni)	0.15
Chrome (Cr)	0.20
Molybdenum (Mo)	0.05
Tin (Sn)	0.03
Vanadium (Va)	0.01
Zinc (Zn)	0.30
Silicon (Si)	0.50
Carbon (C)	1.20
Manganese (Mn)	1.20





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### Shredded Scrap

<b>Specifications</b>	<p><b>Sizing Dimensions:</b> L x W x H</p> <p><i>Maximum (Typically):</i> 0.2m x 0.2m x 0.2m (8" x 8" x 8")</p> <p><i>Minimum:</i> 0.05m x 0.05m x 0.05m (2" x 2" x 2")</p> <p><b>Minimum Density:</b> 800 kg/m<sup>3</sup> (50 lb/ft<sup>3</sup>)</p>
<b>Description:</b>	Homogeneous steel and iron scrap generated by shredding automobile bodies, white goods, unprepared No.1 and No. 2 steel, miscellaneous baling and sheet scrap. Nonferrous material is to be mechanically, magnetically, or hand separated.
<b>Do Not Include:</b>	Engine blocks; tire wire, excessive rust, recuperated tubing, or heat exchanger piping; mill scale; excessive oil filters and nonmetallic fluff; PC tin cans; air bag actuators; rebar; other contaminants such as dirt, rubber, copper wire or plates, wood, grease, oil, HMS. Must not contain scrap from incinerators.
<b>Acceptance Criteria:</b>	Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.
<b>Approval:</b>	Authorized suppliers of key commodities must obtain prior approval from Stelco whenever the product being supplied is different from that currently approved.

Alloy	%, Max
Phosphorus	0.05
Sulfur (S)	0.05
Copper (Cu)	0.25
Nickel (Ni)	0.15
Chrome (Cr)	0.15
Molybdenum (Mo)	0.03
Tin (Sn)	0.03
Vanadium (Va)	0.10
Zinc (Zn)	0.30
Silicon (Si)	0.50
Carbon (C)	0.30
Manganese (Mn)	1.00





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### Slab Crops

<b>Specifications</b>	<p><b>Sizing Dimensions:</b> L x W x H</p> <p><i>Maximum:</i> 0.3m x 0.9m x 1.2m (1ft x 3ft x 4ft)</p> <p><i>Minimum:</i> N/A</p> <p><b>Minimum Density:</b> N/A</p>
<b>Description:</b>	Heavy, clean, "Alloy Free", carbon scrap. Material should be new and free of excessive rust, corrosion, and painted surfaces. Material must be free of torch drippings.
<b>Do Not Include:</b>	Mill scale; cast iron; alloy scrap; obsolete material.
<b>Acceptance Criteria:</b>	Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.
<b>Approval:</b>	Authorized suppliers of key commodities must obtain prior approval from Stelco whenever the product or process is different from that currently approved.

Alloy	%, Max
Phosphorus	0.02
Sulfur (S)	0.02
Copper (Cu)	0.15
Nickel (Ni)	0.15
Chrome (Cr)	0.15
Molybdenum (Mo)	0.03
Tin (Sn)	0.03
Vanadium (Va)	0.10
Zinc (Zn)	0.00
Silicon (Si)	0.50
Carbon (C)	0.30
Manganese (Mn)	2.00





## Rebar

<b>Specifications</b>	<p><b>Sizing:</b></p> <p><i>Loose (Maximum Cut):</i>      0.9 m            (3ft)</p> <p><i>Bundled (Maximum Cut):</i>   0.9 m            (3ft)</p> <p><b>Minimum Density:</b>            800 kg/m<sup>3</sup> (50 lb/ft<sup>3</sup>)</p>
<b>Description:</b>	Rebar scrap from the demolition of concrete structures or from new production. Rebar must be prepared in a fashion that insures efficient magnet handling and compact furnace charging. All rebar must be cut to a maximum of 0.9 m (36") in length. Rebar must be free flowing and be easily handled by grapple or magnet multiple times without becoming tangled. All new material must be free of packaging bags.
<b>Do Not Include:</b>	Badly tangled material or material that would tend to become badly tangled during loading, in amounts that would increase material handling or furnace charging times. Non-ferrous material and Non-metallic parts, copper coated material, cast iron, wire rope, cable, and concrete.
<b>Acceptance Criteria:</b>	Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.
<b>Approval:</b>	Authorized suppliers of key commodities must obtain prior approval from Stelco whenever the product or process is different from that currently approved.

Alloy	%, Max
Phosphorus	0.02
Sulfur (S)	0.05
Copper (Cu)	0.20
Nickel (Ni)	0.15
Chrome (Cr)	0.10
Molybdenum (Mo)	0.02
Tin (Sn)	0.05
Vanadium (Va)	.01
Zinc (Zn)	.03
Silicon (Si)	.50
Carbon (C)	1.0
Manganese (Mn)	1.5





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### Slitter, Bar, Pup and Mandrel Coils

<b>Specifications</b>	<p><i>Maximum Weight:</i> 4 NT</p> <p><i>Maximum Width:</i> 1200 mm (4ft)</p> <p><b>Minimum Density:</b> 800 kg/m<sup>3</sup> (50 lb/ft<sup>3</sup>)</p>
<b>Description:</b>	<p>Slitter, Bar and Mandrel coils are to be generated from the manufacturing sector. Slitter coils must be secured with a minimum of 1 iron band around outside diameter of coil. Slitter, bar and mandrel coils must have 3 iron bands through eye of coil (spaced evenly. Banding must be secure and not move once affixed.) Coils must be able to be handled multiple times with wire banding remaining intact. Supplier is required to follow Stelco strict delivery schedule as a <b>maximum of 2 loads total are permitted per day total</b>. Slitter, Bar, Pup and Mandrel coils must not be intermixed in a single load.</p> <p><b>Delivery Method:</b> Slitter coils must be delivered on a flatbed trailer loaded eye to the sky. If piled on top of each other they must be separated by wood blocking. Mandrel coils must be delivered in end dump trailer. Bar coils must be delivered eye to the side on a flatbed trailer.</p>
<b>Do Not Include:</b>	Material over 4 NT as it is beyond crane capacity.
<b>Acceptance Criteria:</b>	Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.
<b>Approval:</b>	Authorized suppliers of key commodities must obtain prior approval from Stelco whenever the product or process is different from that currently approved.

Alloy	%, Max
Phosphorus	0.03
Sulfur (S)	0.025
Copper (Cu)	0.12
Nickel (Ni)	0.12
Chrome (Cr)	0.12
Molybdenum (Mo)	0.03
Tin (Sn)	0.03
Vanadium (Va)	0.01
Zinc (Zn)	0.30
Silicon (Si)	0.50
Carbon (C)	0.30
Manganese (Mn)	1.00





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### RHOB Coolant

<b>Specifications</b>	<p><b>Sizing Dimensions:</b></p> <p><i>Maximum:</i> 0.006m - 0.025m Thickness x 0.05 m Diameter (0.25" - 1.00" Thickness x 2.00" Diameter)</p> <p><i>Minimum:</i> 0.0127 m Diameter (No minimum thickness) 0.5" Diameter (No minimum thickness)</p> <p><b>Minimum Density:</b> 1500 kg/m<sup>3</sup> (94 lb/ft<sup>3</sup>)</p>
<b>Description:</b>	Clean and dry, all black, free flowing, new mild steel sheet scrap punching, and cut side trim material. Correctly sized nuggetized material.
<b>Do Not Include:</b>	Galvanized material, oil or cutting forming fluids, rust. Must be alloy free material. Please refer to the pictures included with this RHOB Coolant specifications. The pictures that are represented here show acceptable material and non-acceptable material.
<b>Acceptance Criteria:</b>	Material is subject to visual inspection and visual classification, inspection for prohibited items and over/undersized material, mechanical weighing on certified scales, and radiation detection.
<b>Approval:</b>	Authorized suppliers of key commodities must obtain prior approval from Stelco whenever the product or process is different from that currently approved.

Alloy	%, Max
Phosphorus	0.02
Sulfur (S)	0.015
Copper (Cu)	0.10
Nickel (Ni)	0.10
Chrome (Cr)	0.10
Molybdenum (Mo)	0.03
Tin (Sn)	0.03
Vanadium (Va)	0.01
Zinc (Zn)	0.00
Silicon (Si)	0.10
Carbon (C)	0.13
Manganese (Mn)	0.60



**NON-CONFORMING MATERIAL**

<b>RHOB COOLANT—NON-CONFORMING MATERIAL</b>	
<b>Conforming Coolant</b>	<b>Non-Conforming Coolant</b>
<ul style="list-style-type: none"> <li><b>Punching</b> – good size, density, no oxidation, limited risk for plugging</li> </ul>	<ul style="list-style-type: none"> <li><b>Ball bearings</b> – Chemistry issues, potential to jam feeders</li> <li><b>Screws/bolts</b> – coatings, chemistry issues</li> <li><b>Springs</b> – no density, oxidized, chemistry issues</li> <li><b>Materials with low density</b> – no penetration into heat</li> <li><b>Mill Scale</b> – chemistry issues and potential to jam feeders, susceptible to freezing and clumping in winter</li> <li><b>Bird’s eye material</b> – no density</li> <li><b>Large punching</b> - long, ‘C’-shaped, curved or straight – density, shape and plugging issues</li> <li><b>Shred</b> – no density, oxidized material and copper found in shred – chemistry issues</li> </ul>
<b>CONFORMING MATERIAL</b>	
	<p><b><u>Reason for conforming:</u></b></p> <p>Punching- good size, density, no oxidation, limited risk for plugging</p>
<b>NON-CONFORMING MATERIAL</b>	
	<p><b><u>Reasons for non-conforming:</u></b></p> <p>Ball bearing – Chemistry issues</p> <p>Screws/bolts – coatings, chemistry issues</p> <p>Spring – density, oxidized, chemistry issues</p> <p>Copper – Chemistry Issues</p> <p>Low density – no penetration into heat</p>

**NON-CONFORMING MATERIAL**



**Reasons for non-conforming:**

Mill Scale – chemistry and plugging in the alloy system, susceptible to freezing and clumping in winter

Bird's eye material – no density



**Reasons for non-conforming:**

Ball bearing – chemistry issues, potential to jam feeders



**Reasons for non-conforming:**

Large punching – density, shape and plugging issues ( 'C' shaped or curved)



**Reasons for non-conforming:**

Oversized, long, curved or straight – causes plugging and has no density

**NON-CONFORMING MATERIAL**



**Reasons for non-conforming:**

Oversized material – no density, plugging issues



**Reasons for non-conforming:**

Shred – no density, oxidized material risk for chemistry, copper found in shred risk for chemistry



**Reasons for non-conforming:**

Oxidized material, chemistry issues



**Reasons for non-conforming:**

Oversized material – no density, plugging issues

## HEAVY MELT—NON-CONFORMING MATERIAL

### Roller

- Fe 66.3%
- Cr 22.5%
- Ni 5.6%
- Mo 3.1%
- Mn 1.7%
- Si 0.36%
- Cu 0.20%
- V 0.05%
- S 0.05%
- P 0.02%



### Machine Base

- Fe 95.9%
- Si 2.65%
- Mn 0.94%
- S 0.27%
- Cr 0.05%
- Ti 0.05%
- V 0.04%
- Cu 0.03%
- Mo 0.03%



**SHRED—NON-CONFORMING MATERIAL**

Dirt/Mill Scale



Fluff



**BUSHELING—NON-CONFORMING MATERIAL**

Fabric Bag





**PRIME/INDUSTRIAL BUNDLES—NON-CONFORMING MATERIAL**

Wood, Plastic, Rebar, etc.



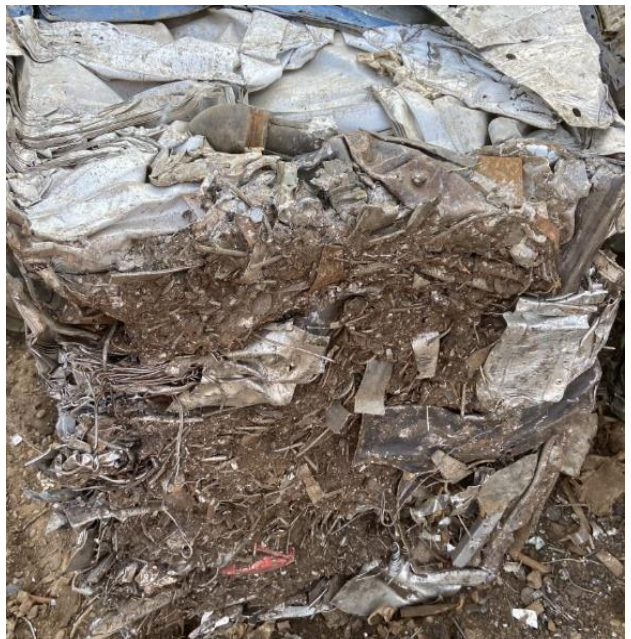
Wood, Plastic, Rebar, etc.



Wood, Plastic, Rebar, etc.



Wood, Plastic, Rebar, etc.



**DEALER BUNDLES—NON-CONFORMING MATERIAL**

**Plastic, Rubber, Dirt**



**Plastic, Rubber, Dirt**

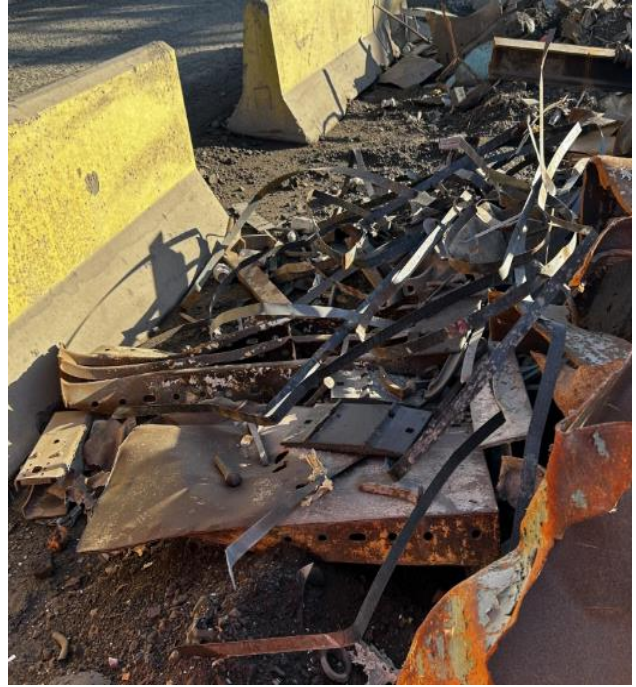


**PLATE AND STRUCTURAL—NON-CONFORMING MATERIAL**

Shredder



Banding



Pit Scrap



Slag



**MISCELLANEOUS—NON-CONFORMING MATERIAL**

**Engine/Motor**



**Pressurized Cans**



**Tank**



**Full Hydraulic Cylinder**



**High Alloy Pin**



**Insulation and Debris**



**MISCELLANEOUS—NON-CONFORMING MATERIAL**

**Counterweights**



**Final Drive**



**MISCELLANEOUS—NON-CONFORMING MATERIAL**

Differential



Rubberized Roller



Rotor (Cast)



Electric Motor



**MISCELLANEOUS—NON-CONFORMING MATERIAL**

Graphite



Oxygen Tank



**HAZARDOUS MATERIAL—STRICTLY PROHIBITED**

Lead



Lead-Containing Counterweight

